

# LONDON BOROUGH OF ENFIELD

## PLANNING COMMITTEE

**Date:** 15 December 2020

**Report of**  
Head of Planning

**Contact Officer:**  
Andy Higham  
Claire Williams  
Eloise Kiernan  
Tel No: 020 8132 2130

**Ward:**  
Edmonton Green

**Ref:** 17/05384/FUL

**Category:** Major Application

**LOCATION:** Edmonton Methodist Church, 300 Fore Street, London, N9 0PN

**PROPOSAL:** Redevelopment of site including the erection of a 4 storey block of 24 self-contained flats with parking at ground floor and partial demolition of existing church for the erection of a new 3 storey Church building involving vehicular access off Brettenham Road.

**Applicant Name & Address:**

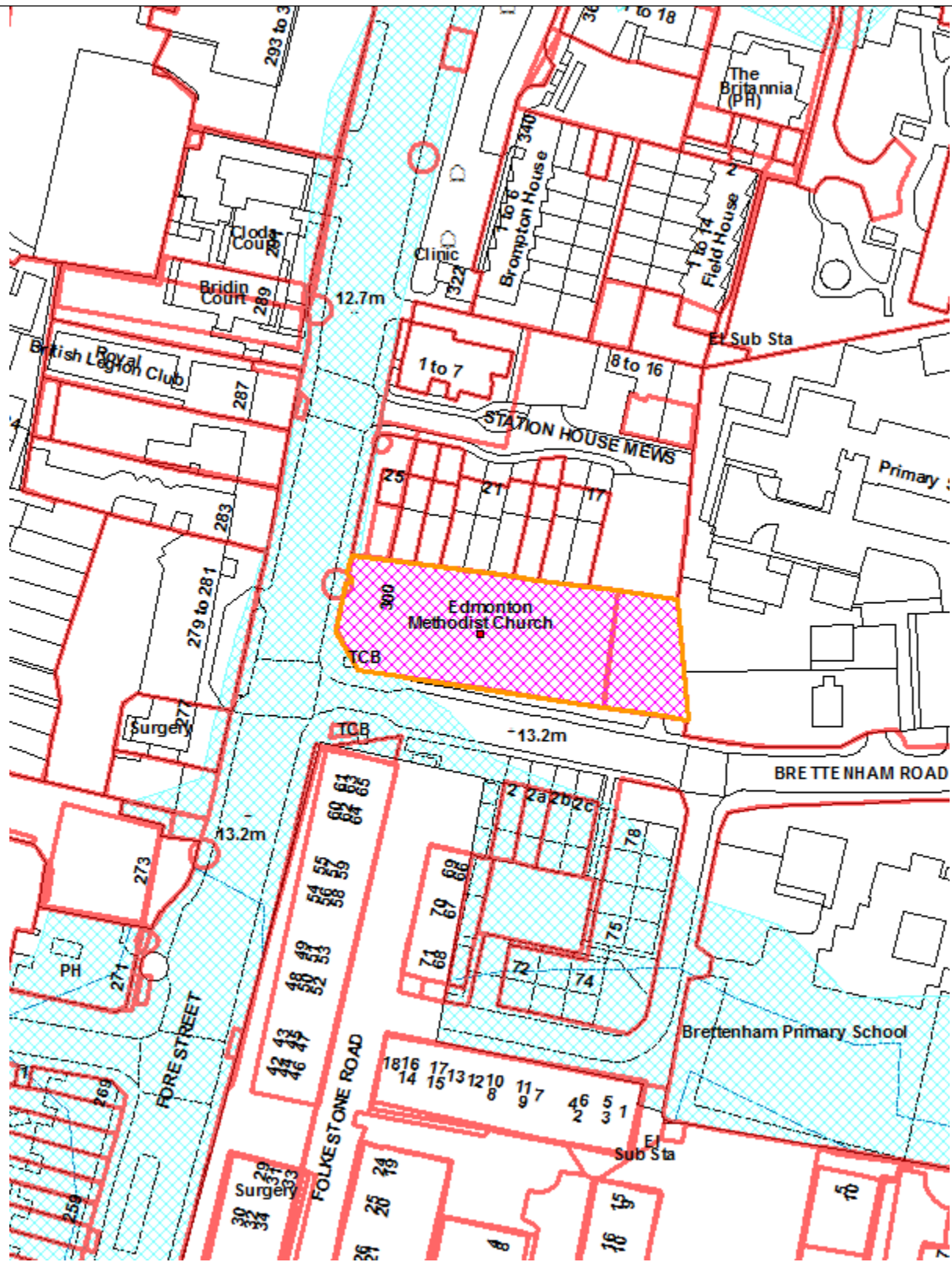
Edmonton Methodist Church  
300 Fore Street  
London  
N9 0PN

**Agent Name & Address:**

Mr Haydn Jones  
Saville Jones Consultancy  
74 Victoria Road  
Worthing  
BN11 1UN

**RECOMMENDATION:**

That subject to the recommendations as set out in the report, the Head of Development Management / the Planning Decisions Manager subject to the completion of a section 106 legal agreement be authorised to **GRANT** planning permission subject to conditions.



**1. Note for Members**

- 1.1 The application is being reported to the Planning Committee for determination because it is a Major development.

**2. Recommendation/Conditions**

- 2.1 That, the Head of Development Management/Planning decisions Manager, be authorised to GRANT planning permission subject to the following conditions:

1. Time Limit
2. Approved Plans
3. Full details, specifications, samples of all External Materials, including detailed drawings scaled 1:20 with 1:5 sections
4. Contamination assessment
5. Air Quality Assessment
6. Sound Insulation.
7. Landscaping
8. Biodiversity enhancements
9. Hard surfacing
10. Enclosure (including privacy screens to balconies)
11. Construction Management Plan
12. External Lighting
13. Energy Statement
14. EPC's
15. SuDS Strategy
16. SuDS Verification
17. Water Efficiency
18. Considerate Constructors
19. Green Roof
20. Refuse Storage
21. Cycle Storage
22. Site Waste Management Plan

23. Redundant Access
24. New Access
25. Tree protection
26. BREEAM Accreditation
27. Accessible housing – compliance with Part M4 (2) Building Regulations
28. PD Restriction – church only
29. Details of privacy screens
30. Hours of use

### **3. Executive Summary**

- 3.1 This application seeks approval for a scheme involving the redevelopment of site including the erection of a 4 storey block of 24 self-contained flats with parking at ground floor and partial demolition of existing church for the erection of a new 3 storey Church building involving vehicular access off Brettenham Road
- 3.2 The scheme is considered acceptable for the following reasons:
  - i. The improvements and rebuilding of the church provide public benefit and a valuable community asset;
  - ii. It would retain a locally listed heritage asset within the Fore Street Conservation Area;
  - iii. The improved design and sustainability credentials would improve both the visual amenity and character of the area while contributing towards environmental objectives within the Borough;
  - iv. It would provide good quality housing stock to the Borough within a sustainable location;
  - v. It would not be detrimental to residential amenities;
  - vi. It would not compromise highway safety;

### **4. Site and Surroundings**

- 4.1 The application site is located on the eastern side of Fore Street and northern side of Brettenham Road and currently features a detached building which currently serves a church (D1 use) facility. The site area is approximately 0.18 hectares.
- 4.2 The application site comprises a landmark building of Arts and Crafts style within the boundaries of the Fore Street Conservation Area and identified as making a positive contribution to the area. The site is adjacent to the Police Station to the west, which is a listed building. The existing building is part single and part two storeys in height.
- 4.3 The surrounding area typically comprises residential dwelling located off Fore Street. The rear gardens of properties sited at Station House Mews abut the site to the north and flatted developments are located to the south.



- 4.4 The application site is located within Flood Zone 2 and identified as a Site of Archaeological Interest.

## **5. Proposal**

- 5.1 The applicant seeks full planning permission for the redevelopment of site including the erection of a 4 storey block of 24 self-contained flats with parking at ground floor and partial demolition of existing church to facilitate the erection of a new 3 storey Church building involving vehicular access off Brettenham Road
- 5.2 The application has been amended during its determination period with the input of both the urban design and heritage officers. The scheme has subsequently been redesigned to accommodate an increase from 12 to 24 residential units and the heritage asset has been retained and incorporated into the redevelopment.

## **6. Relevant planning history**

- 6.1 17/00817/PREAPP - Proposed redevelopment of site including the demolition of the existing building and erection of a new Church together with 35 self-contained flats, (comprising 9 x 3-bed, 14 x 2-bed flats, 12 x 1-bed), together with basement car parking and communal external space and roof garden – response issued
- 6.2 P13-01254PLA - Widening of existing 2 x vehicle access, installation of replacement gates and metal fencing – granted with conditions
- 6.3 P13-03720NMA - Non material amendment to P13-01254PLA to allow the proposed sliding gate to be changed to a double inward opening gate – agreed.

## **7. Consultation**

### 7.1 Statutory and non-statutory consultees

#### Internal

- 7.2 Traffic and Transportation – No objections subject to conditions and a S106.
- 7.3 SuDS – No objections as FRA was submitted subject to SuDS conditions.
- 7.4 Environmental Health – No objections subject to conditions

#### External

- 7.5 Thames Water – No objections

#### Public

- 7.6 Consultation letters were sent to 122 neighbouring properties. The application was also advertised in the local paper and by site notice. Further re consultation on the revised information was undertaken. Four representations

objecting to the development were received. The main issues raised in summary were:

- Increase in traffic;
- Loss of parking
- Strain on existing community facilities;
- Close to adjoining properties;
- Inadequate access;
- Affects local ecology;
- Conflicts with Local Plan;
- Increase of pollution;
- Loss of light;
- Loss of privacy;
- Out of keeping with character of the area;
- Overdevelopment of the site;
- Potential contamination of the land;
- Impact on existing heritage assets at the site and the listed Police Station, contrary to Local Plan.

7.7 Additionally, there were 10 representations in support as well as a Statement of Support with signatures submitted by the Minister, Rev Valentin Dedji.

## **8. Relevant Planning Policies**

### **8.1 Development Management Document**

DMD1	Affordable Housing
DMD3	Mix of Decent Sized Homes
DMD6	Residential Character
DMD8	New Residential development
DMD9	Amenity Space
DMD10	Distancing
DMD16	Provision of New Community Facilities
DMD31	Development Involving Tourism and Visitor Accommodation
DMD37	Achieving High Quality and Design-Led Development
DMD44	Conserving and Enhancing Heritage Assets
DMD45	Parking Standards and Layout
DMD46	Vehicle crossovers and dropped kerbs
DMD47	New Roads, Access and Servicing
DMD48	Transport Assessments
DMD50	Environmental Assessment Methods
DMD51	Energy efficiency standards
DMD53	Low and zero carbon technology
DMD56	Heating and cooling
DMD57	Responsible sourcing of materials, waste minimisation and green procurement
DMD58	Water efficiency
DMD61	Managing Surface Water
DMD64	Pollution Control and Assessment
DMD65	Air quality
DMD66	Land contamination and instability
DMD68	Noise
DMD69	Light Pollution
DMD71	Protection and Enhancement of Open Spaces

DMD76	Wildlife Corridors
DMD77	Green Chains
DMD78	Nature Conservation
DMD79	Ecological Enhancements
DMD80	Trees on Development Sites
DMD81	Landscaping

## 8.2 Core Strategy

CP2	Housing supply and new homes
CP3	Affordable Housing
CP4	Housing quality
CP5	Housing type
CP9	Supporting community cohesion
CP11	Recreation, leisure, culture and arts
CP12	Visitors and Tourism
CP20	Sustainable energy use and energy infrastructure
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP24	The road network
CP25	Pedestrians and cyclists
CP26	Public transport
CP28	Managing flood risk through development
CP29	Flood management infrastructure
CP30	Maintaining and improving the quality of the built and open environment
CP31	Built and landscape heritage
CP32	Pollution
CP34	Parks, playing fields and other open spaces
CP36	Biodiversity

## 8.3 London Plan (2016)

2.2	London and the wider Metropolitan area
2.6	Outer London: vision and strategy
2.7	Outer London: economy
2.8	Outer London: transport
2.16	Strategic outer London development centres
3.1	Ensuring equal life chances for all
4.6	Arts, culture, sport and entertainment provision
5.1	Climate change mitigation
5.2	Minimising carbon dioxide emissions
5.3	Sustainable design and construction
5.6	Decentralised energy in development proposals
5.7	Renewable energy
5.10	Urban greening
5.11	Green roofs and development site environs
5.12	Flood risk management
5.13	Sustainable drainage
6.3	Assessing the effects of development on transport capacity
6.9	Cycling
6.10	Walking
6.11	Smoothing traffic flow and tackling congestion
6.12	Road network capacity
6.13	Parking

7.1	Building London's neighbours and communities
7.2	An inclusive environment
7.3	Designing out crime
7.4	Local character
7.5	Public realm
7.6	Architecture
7.8	Heritage assets and archaeology
7.14	Improving air quality
7.15	Reducing noise and enhancing soundscape
7.19	Biodiversity and access to nature
7.21	Trees and woodlands

#### 8.4 Draft London Plan

8.4.1 The Intend to Publish London Plan was published on 9 December 2019. The Secretary of State for Housing, Communities and Local Government has responded and directed that the Plan cannot be published until the Directions he has listed are addressed. He has raised concerns that there were a number of inconsistencies with national policy and missed opportunities to increase housing delivery. Directions relevant to this application include.

8.4.2 In the circumstances, it is only those policies of the Intention to Publish version of the London Plan, that remain unchallenged to which weight can be attributed.

D4 Delivering good design  
D5 Inclusive design  
D8 Public Realm  
G6 Biodiversity and access to nature  
G7 Trees and woodlands  
SI1 Improving air quality  
SI13 Sustainable drainage  
T1 Strategic approach to transport  
T3 Transport capacity, connectivity and safeguarding  
T4 Assessing and mitigating transport impacts  
T5 Cycling  
T6 Car Parking

#### 8.5 Other Relevant Policy

- National Planning Policy Framework (2019)
- National Planning Practice Guidance (2019)

#### 8.6 Other Material Considerations

- Fore Street Angel Conservation Area Character Appraisal (2015)
- GLA Housing SPG (2016)
- Nationally Described Space Standards

### 9. **Analysis**

9.1 The main issues for consideration regarding this application are as follows:

- Principle of the Development including Impact on Heritage Assets and Fore Street Conservation Area and Community Use;
- Design and Character;
- Standard of Accommodation;
- Housing Mix;
- Affordable Housing;
- Neighbouring Amenities;
- Traffic and Transportation;
- Trees and Biodiversity;
- Flooding and Drainage;
- Sustainability; and
- Contamination.

## 9.2 Principle of the Development

### Heritage Assets

#### Edmonton Methodist Church

9.2.1 Edmonton Methodist Church comprises an arts and crafts building circa 1927 which fronts Fore Street, with an earlier hall to the rear. Both buildings lie within the boundaries of the Fore Street Conservation Area. The frontage building is noted in the Fore Street Conservation Area Character Appraisal as being a landmark building and also forms a focal point in the conservation area. Stylistically, it has a symmetrical 3 storey, 3 bay frontage with projecting side gables of red brick with stone dressings, and yellow stock to rear wall. A central 5 light variant on the Diocletian window with decorative stone surround can be seen to the central bay. Timber framed casement windows exist throughout with leaded lights and tiled cills. Creasing tiles to quoins and window heads. Exposed brick end stacks. Slated hipped roof over.

9.2.2 The rear hall is noted as making a positive contribution to the conservation area. Originally the Sunday School, it was constructed in the late 19<sup>th</sup> century of London Stocks with large arched windows and copper ventilation cowls. The former vestibule is now the main entrance and features later entrance canopy and rendered walls. A slightly later club room and kitchen face Brettenham Road, of brick with replacement uPVC windows. The original forecourt has been given over to hardstanding with hoop top railings to boundary.

9.2.3 Edmonton Methodist Church forms part of the loose group of landmark late 19<sup>th</sup>/early 20<sup>th</sup> century former public and religious buildings, the former Police Station, 1905, by JD Buter, the inter-war Ambulance Stations nos.305-309 and the former library. Key views are afforded along Park Road and northwards along Fore Street.

#### Fore Street Conservation Area

9.2.4 The Act defines conservation areas as 'areas of special architectural or historic interest, the character or appearance of which it is desirable to preserve or enhance'. The significance and special character of the Fore Street Conservation Area essentially is that it comprises one of the oldest routes north from London and is characterised by its rich and varied building

stock which ranges from the early 19<sup>th</sup>-century to the present day, arranged in a historic linear settlement pattern. Its diversity is part of its significance and structures 'should be seen as part of the street as a whole' (2.5.2) more than in terms of particular stylistic or historic groupings: 'each building tends to be different from its neighbours' (2.6.10).

9.2.5 Rectilinear landscape division was established in the area during the Roman period with settlement developing along similar lines, most notably the spine route which was formed by Ermine Street. Fore Street represents a medieval diversion from the Roman road, beginning at what is now the borough boundary. Despite some historic buildings being lost as part of redevelopment in the 1950s, the linear settlement pattern can still clearly be seen today. The surviving historic areas of Fore Street which make a positive contribution to its character are defined by a strong street frontage and sense of enclosure. Although some historic buildings were lost as part of the redevelopment in the 1950s and preparations for road widening schemes (which were never implemented), the linear settlement pattern can still clearly be seen.

9.2.6 The overall character of this part of the Conservation Area has been shaped in three key phases:

1. Ribbon development of suburban, mainly residential, development - which grew up incrementally from the 17th century along the main road out of London and led to both frontages being fully developed by around 1870.
2. Expansion: the development of the fields behind the frontage buildings for large-scale suburban housing with Fore Street becoming a local commercial and retail centre to serve it- particularly at the north and south ends of the character area. The central area remained residential until the late 20th century, interspersed with (former) public buildings like the Library, Police Station and Ambulance Station. This phase was at its peak between 1890-1914, continuing into the inter-war years. The transition of Fore Street to commercial uses was achieved both by conversion of existing houses, often with 'bungalow fronts' built out over former front gardens, and with new larger buildings. Architecturally, the new buildings form two distinct groups, those built around 1900, and those from the inter-war years.
3. Post-war intervention by public authorities using compulsory purchase to facilitate comprehensive redevelopment of large areas, often linked to road 'improvements' or precipitated by bomb damage. Public houses and churches were often retained, primarily because of the high cost of compensation on the basis of 'equivalent reinstatement' (the cost of replacement buildings of similar size and quality), which applied to them.

#### Impact on the Heritage Assets and Conservation Area

9.2.7 The frontage building has been identified as a landmark building in the Fore Street Conservation Area Character Appraisal and is therefore deemed to make a positive contribution to the character and appearance of the Conservation Area. Of the rest of the group, the current church hall is identified as building making a positive contribution to the Conservation Area and other structures are identified as neutral. In planning terms, it is therefore considered that both the former hall and the frontage building be identified as non-designated heritage assets. A non-designated heritage asset can be a

building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions.

9.2.8 A landmark is defined as:

*A prominent or conspicuous object on the land that serves as a guide..... a distinguishing landscape feature marking a site or location..... a building or other place that is of outstanding historical, aesthetic, or cultural importance, often declared as such and given a special status (landmark designation) , ordaining its preservation, by some authorizing organization*

9.2.9 The NPPF sets out how harm to heritage assets should be approached. Harm to heritage assets is measured as 'less than substantial' or 'substantial harm'. Irrespective of whether harm is 'less than substantial' or 'substantial' the NPPF (backed up by case law) requires the Local Planning Authority to attach 'great weight' to harm. The loss of a landmark building within a conservation area would normally cause a high level of harm and a comparative 'no harm' scheme/ options appraisal would normally need to be submitted, as part of the application process. In this instance, any harm was of particular concern because of the challenging condition of the Conservation Area, noted in the appraisal document, and the associated implications for the heritage asset itself (the Fore St Conservation Area).

9.2.10 Following extensive and detailed discussions, the applicants were asked to submit an options appraisal for the site in line with paras 193 and 197 of the NPPF. The options appraisal considered three options for the site, one of which constituted no harm. Option 2, which sees the retention of the landmark frontage building and loss of the building making a positive contribution to the character of the conservation area was ultimately taken forward, as although it would result in the loss of the building noted as making a positive contribution to the Conservation Area, it was considered that the replacement building was of an acceptable design that would preserve the character of the Conservation Area and that it allowed for the retention of landmark the frontage.

- 1. Retention of the front "landmark" and rear positive contributor (no harm);*
- 2. Retention of the "landmark" element but demolition of the rear building;*
- 3. Loss of landmark and rear positive contributor (total loss of both historic buildings).*

9.2.11 Recent case law provides important clarification on the way in which applications concerning the demolition of non-designated heritage assets (NDHA) in Conservation Areas should be handled. The Dorothy Bohm v SSCLG [2017] EWHC 3217 Judgment clarifies that just because something is a 'positive contributor', so long as it is not designated in itself, a Local Planning Authority should normally not automatically conclude that it cannot be demolished/ redeveloped until it has assessed it in comparison with the potential enhancements of a proposed development. Importantly, this implies that the demolition of an NDHA in a Conservation Area cannot be treated as harm to a designated heritage asset in isolation, but that the scheme as a whole needs to be considered, with the demolition being just one factor in this.

9.2.12 The judgement effectively holds that the demolition of an NDHA in a Conservation Area should not be regarded in the same way as if it were the

designated asset itself. Even if the existing building makes a positive contribution to the Conservation Area and would be completely lost, this does not mean that the Conservation Area would inevitably be harmed. If the replacement building is of an acceptable design that would preserve the character of the Conservation Area, then it is considered that no harm to the Conservation Area would arise as a result of the proposals.

9.2.13 On that basis, the loss of a positively contributing NDHA does not automatically mean that harm must arise to the Conservation Area. The key questions to ask are therefore; whether great weight has been given to the conservation of the designated heritage asset (i.e. the Conservation Area), and whether the replacement building will preserve the character of the Conservation Area.

9.2.14 Although the proposals will incur the loss of the rear hall building which makes a positive contribution to the character of the Conservation Area, its replacement with a building of an acceptable design that preserves the landmark frontage building and the character of the Conservation Area, means that no harm would arise to the designated heritage asset as a result of the proposals in line with paras 193 and 197 of the NPPF.

9.2.15 Having regard to the above, the partial demolition and introduction of a replacement building that preserves the heritage asset and Fore Street Conservation Area as a whole is therefore considered acceptable, having regard to the advice contained within the National Planning Policy Framework, Planning (Listed Buildings and Conservation Areas) Act, 1990, policy CP31 of the Core Strategy, policy 7.8 of the London Plan and policy DMD44 of the DMD and the aims and objectives contained within the Fore Street Conservation Area Character Appraisal.

#### Re Provision of the Community Facility

9.2.16 Policy CP11 of the Core Strategy relates to Recreation, Leisure, Culture and Arts; the Council will seek to protect existing assets and provision, and promote and encourage the increased use of recreation, leisure, culture and arts facilities in the Borough. Additionally, policy DMD17 of the Development Management Document relates to the protection of community facilities within the Borough, together the policies are supportive of improvements to existing facilities.

9.2.17 It is noted that the existing facility would be retained and provided with improved facilities to maintain the same level of public provision and accessibility to cater for the local community, which is welcomed. The ground floor would feature a large foyer with coffee bar and reception counter as well as Prayer Chapel and Vestry and a sanctuary with seating for 289 people. The first floor would comprise the Minister's office, a meeting room, a community room and main hall. The main hall has been designed to serve a traditional church hall but would also allow for sports facilities to cater for the church and community. The second floor features a further two meeting rooms which again provide further opportunities for the community use, which are again deemed to be positive features alongside the church facility itself, having regard to Policies CP11 of the Core Strategy and DMD17 of the Development Management Document.



### 9.3 Design and Character

- 9.3.1 The National Planning Policy Framework specifies that design policies should concentrate on guiding the overall scale, density, massing, height, landscape, layout and materials of developments in regard to neighbouring buildings and the local area more generally. Additionally, particular architectural styles or tastes need not be imposed as this could hinder innovation, however developments should seek to promote or reinforce local distinctiveness. Furthermore, permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of the area, and the way it functions.
- 9.3.2 Following various meetings and input from urban design and heritage teams within the Planning Service, the building has been redesigned and additional height has been added to integrate the building with the existing heritage assets and surrounding street scene.
- 9.3.3 The proposed new build element is of a contemporary ecclesiastical style and incorporates a corner tower element with pre-patinated zinc to provide an additional focal point to mark the corner element with Brettenham Road. The existing heritage asset is of an art deco design. The proposed contemporary architectural approach thereby marries the old and new elements together and thus the site remains a focal point as it retains the old landmark whilst creating a new landmark element. The proposed design also incorporates a glazed link to provide visual permeability and separation between both buildings as well as a good transition between the buildings, which therefore allowed the introduction of increased height to the rear element away from the heritage asset.
- 9.3.4 The street scene predominantly comprises 3-4 storey buildings and as such the ridge height was not considered to be excessive in regard to the surrounding character, particularly given that the bulk and massing were broken up by the varying eaves and ridge heights afforded by the glazed link and art deco heritage asset. These provided good transition between building heights whilst incorporating a sensitive design to preserve and enhance the heritage asset.
- 9.3.5 The overall design incorporated pyramidal rooflights to the front elevation, which would not normally be deemed acceptable, however further discussions with the applicant ensured that they would not be visible from pavement level thus preserving the relationship with the landmark building.
- 9.3.6 Further improvements to the public realm were also secured, which involved the shunting of the building to allow appropriate greening to the frontage. This is welcomed in heritage terms as it creates an active frontage whilst providing improvements to the conservation area and further greening to an otherwise urban environment.
- 9.3.7 It is therefore considered that the overall design is considered acceptable and would integrate satisfactorily with the surrounding street scene subject to appropriate conditions pertaining to details of all proposed materials, including a brick sample panel (showing brick type, bond and mortar) to be erected on site, and detailed drawings at 1:20 or larger with 1:5 sections showing the

proposed junction between the existing building and new development at roof level, roof (eaves and parapet detail) and any new or replacement doors (including jambs, frame, door case, door furniture) and windows (including cills, reveals, heads and window furniture).

#### 9.4 Standards of Accommodation

9.4.1 Policy 3.5 of the London Plan, as detailed in Table 3.3 stipulates the minimum space standards for new development. The proposed units will be expected to meet and where possible exceed these minimum standards as well as the design criteria in the London Housing SPG. The nationally described space standard (NDSS) was introduced on 25 March 2015 through a written ministerial statement as part of the New National Technical Housing Standards.

9.4.2 The floorspace required for each unit is as follows:

1b2p – 50 sq.m  
2b3p – 61 sq.m  
2b4p - 70 sq.m

9.4.3 The submitted floor plans indicate that all of the proposed units would meet the minimum standards with a suitable internal layout, including predominately dual aspect layouts. It is therefore considered that the proposed units would provide an acceptable level of accommodation, having regard to policy 3.5 of the London Plan and the guidance contained within the Housing SPG (2012).

9.4.4 Additionally, DMD9 of the Management Document indicates that the following minimum private amenity space standards for individual units alongside communal amenity space:

1b2p – 5 sq.m  
2b3p – 6 sq.m  
2b4p – 7 sq.m

9.4.5 Each unit would have access to communal amenity space as well as its own designated amenity space with balcony and thus would provide a suitable form of accommodation, having regard to policies DMD8 and DMD9 of the DMD.

#### 9.5 Housing Mix

9.5.1 Policy DMD3 of the DMD and CP5 of the Core Strategy seek to provide a suitable housing mix of 20% 1 and 2-bed, 15% 2-bed, 45% 3-bed and 20% 4+bed. The proposed unit mix is as follows:

<b>Units</b>	<b>No. Units</b>
1 Bed, 2 person (Flat)	19
2 Bed, 3 person (Flat)	3
2 Bed, 4 person (Flat)	2

<b>Total</b>	<b>24</b>
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9.5.2 The development would provide a mix of 80% one beds and 20% two beds, however, on balance, the net gain of residential assets alongside an improved community asset, given the significant heritage considerations, in this instance is considered acceptable.

9.5.3 It should be recognised that there is a need for all types of housing across the borough including smaller 1 and 2 bed units and when looking at the planning priorities and merits of this scheme the proposed housing mix is on balance acceptable. Furthermore, the supporting Planning Statement specifies that two units would be Wheelchair User Dwellings and the remaining units (22) would be accessible and adaptable homes, which provides for the whole community, including those that are mobile impaired.

#### 9.6 Affordable Housing

9.6.1 Planning policy states that development should provide the maximum amount of affordable housing that is viable. In this instance, it is concluded that the scheme cannot sustain the delivery of any onsite affordable housing or obtain any financial contribution to deliver off site affordable housing.

9.6.2 A Viability Appraisal was submitted as part of the proposals and this was reviewed by an experienced independent viability consultant.

#### 9.7 Neighbouring Amenity

9.7.1 Policy 7.6 of the London Plan Policy states that buildings should not cause unacceptable harm to residential amenity, including in terms of privacy and overshadowing. Additionally, policies DMD6 and DMD8 of the DMD ensure that residential developments do not prejudice the amenities enjoyed by the occupiers of neighbouring residential properties in terms of privacy, overlooking and general sense of encroachment - the principles contained in this policy have been applied in this case given the relationship to residential properties. Furthermore, Policy CP30 of the Local Plan seeks to ensure that new developments have appropriate regard to their surroundings, and that they improve the environment in terms of visual and residential amenity.

9.7.2 The properties most impacted on by the development would be those abutting the site at Station House Mews. In order to respect the houses to the north of the site (in Station House Mews), the building steps back away from the northern boundary as it increases in height. This is in line with BRE Report 209 – Daylight and Sunlight and reduces any adverse effects from overshadowing.

9.7.3 A Daylight/Sunlight Report was submitted as part of the applications and this considers the impact on this particular terrace in regard to windows as well as the garden areas. The main criteria used in this analysis to show compliance are the Annual Probable Sunlight Hours and Vertical Sky Component tests. The report concludes that the effect on VSC is within the 80% guidance value in all cases and therefore there will be no adverse impact on neighbouring residents in terms of daylight. In regard to sunlight, it has been demonstrated that all windows meet the BRE criteria by virtue of retaining 80% of their

existing value. The neighbouring gardens also retain in excess of 80% of their current values. All neighbouring gardens would retain at least 2 hours or more of direct sunlight on March 21<sup>st</sup> in excess of 50% of the garden area. There would therefore be no adverse impact on sunlight receipt to neighbouring properties.

- 9.7.4 It is therefore concluded that the proposed bulk, scale and massing would not be overbearing or give rise to an unacceptable loss of sunlight/daylight or outlook to neighbouring occupiers, having regard to policy DMD8 of the DMD.
- 9.7.5 Additionally, the layout has been carefully designed to exclude windows in the eastern elevation to minimise the potential for overlooking, having regard to policies DMD8 and DMD10 of the DMD. Furthermore, appropriate conditions could be attached to secure screens to the balconies where necessary.
- 9.8.1 Traffic and Transportation
- 9.8.2 Policy DMD45 relates to car parking, cycle provision and parking design. DMD47 states that new development proposals will need to demonstrate that enough space for servicing, circulation and access to, from and through the site is provided. All developments must be fully accessible to pedestrians and cyclists and assist with general permeability within an area and the current factory does not provide this. London Plan policy 6.13, DMD policy 45 (Parking Standards and Layout) and 47 (Access, New Roads and Servicing) states that operational parking for maintenance, servicing and deliveries is required to enable a development to function.
- 9.8.3 Fore Street is a principal road and Brettenham Road is unclassified with a PTAL of 5 (very good), which indicates that the site is well connected to public transport services. The existing site has a vehicle crossover onto the A1010, Fore Street and a vehicle access off Brettenham Road providing access to parking at the rear of the site. The site is located in the South Edmonton Controlled Parking Zone (CPZ) which is an Event day only CPZ, opening times: noon to 9 pm. There are also other waiting and loading restrictions present in the vicinity of the proposal site.

#### *Parking*

- 9.8.4 The development would provide 24 units (19 x 1b2p, 3 x 2b3p and 2 x 2b4p) combined with the new church and associated rooms with a gross internal area totalling 1332.4 m<sup>2</sup> (the Sanctuary and Main Hall combined have 439 seats).
- 9.8.5 Public Transport Accessibility Level (PTAL) is a widely adopted methodology in Greater London for quantifying a site's accessibility to public transport and is considered to be a usable measure of relative accessibility to public transport at any location within a London borough and provides a general comparison of a site's accessibility relative to another. The site has a PTAL of 5 which indicates that access to frequent public transport services is very good.
- 9.8.6 The Mayor wishes to see an appropriate balance being struck between promoting new development and preventing excessive car parking provision that can undermine cycling, walking and public transport use.

Maximum residential parking standards:

- 1-2 beds = less than 1 parking space per unit

- 9.8.7 A total of six car parking spaces are proposed for the whole development with five undercroft car parking spaces allocated for the apartments and the Church: these spaces are to be accessed via a new access point off Brettenham Road.
- 9.8.8 This parking area consists of two disabled, blue badge holder spaces and an active electric vehicle charging space. Two passive electric vehicle spaces are also to be included. The side parking area is to be located behind a roller gate. As gates are to be provided on this access, a condition is to be imposed to ensure they are sited a minimum distance of 5m from the carriageway edge to enable vehicles to pull clear of the highway whilst the gates are opened or closed, to avoid vehicles having to wait/stop/park on the adjoining public highway.
- 9.8.9 One additional external disabled space for the Church is to be located at the front of the Church. This is similar to the existing arrangement and the parking area is to be accessed via the existing retained vehicle crossover off the A1010, Fore Street.
- 9.8.10 It should be noted that no stopping, waiting, pick-up/drop off is permitted from Fore Street as a result of the public realm/cycle improvements.
- 9.8.11 The existing Methodist Church has parking for 12 vehicles at the front and front side of the building with another parking area with capacity for approximately 16 vehicles to the rear. Google Street View images from March 2019 shows that there was a high demand for these spaces and shows double parking taking place (cars blocking others in).
- 9.8.12 Given there is only the Tottenham Hotspur Event Day controlled parking zone covering the site (which does not consistently operate over the likely periods of peak demand for the site), the Council would expect a contribution of £25,000 to cover the cost of consulting on and, if necessary, implementing parking controls in the area. This could be contingent on the results of car parking surveys of the area showing a significant reduction in parking capacity arising from this development.

#### Trip Generation and Parking Surveys

- 9.8.10 A Transport Statement (TS) has been submitted as part of the accompanying documents. Given the existing use as a place of worship (D1) on the site and as it is only undergoing reconfiguration, the assessment excluded this element of the proposal from the trip generation assessment.
- 9.8.11 Trip rates have been derived from TRICS (Trip Rate Information Computer System). TRICS is the national system of trip generation analysis for the UK and Ireland, an essential method of measuring the likely transport generated by new developments. Following calculation and analysis of forecast trips the TS concludes: *"...that the proposals represent no material issues in highway or transport terms."*
- 9.8.12 On-street parking surveys were undertaken within a 100m, 200m, 300m and 400m walking distance of the site. An independent survey company was used, and surveys were undertaken on the following dates and time periods:
- Thursday 14th July, 2016; 08:00 – 19:00
  - Sunday 17th July, 2016; 08:00 – 18:00

9.8.13 The survey results concluded that cars associated with this development could utilise the local highway network and be accommodated within the existing on-street car parking capacity, which is therefore considered acceptable.

#### Cycle Parking

9.8.14 The proposed development would introduce a total of 54 new cycle spaces for the development located in convenient and easy to use locations on site. Cycle storage and racks, capable of accommodating a total of 40 bicycles would serve the residential units and cycle racks for 14 bikes are also included for the church congregation to use. These are located on the north side of the building at ground level.

9.8.15 The London Plan cycle parking standards are as follows:

Table 6.3 Cycle Parking minimum standards:

Land use	Long-stay
Dwellings (all)	1 space per studio and 1 bedroom unit
	2 spaces per all other dwellings

Note: In addition, the applicant must provide short-stay cycle parking in an accessible location: 1 space per 40 units, with a minimum provision of 2 spaces.

Land use		Long-stay	Short-stay
D1	Church	1 space per 8 staff	1 space per 100 sqm

9.8.16 The design of the bike store should ensure that it is big enough to accommodate cycles with stands/racks, lockable (by an access fob/card or BS mortice lock), allowing both the frame and at least one wheel to be secured. The plans provided should include detailed designs of the bike store, including dimensions, materials of the bike racks and materials of the bike store and also showing the proposed racks / stands in the store. Guidance is set out in the London Cycle Design Standards.

9.8.17 The number and location of cycle storage is considered acceptable and further details could be secured by an appropriate condition, should the scheme be granted.

#### Refuse and Recycling

9.8.18 Policy DMD 47 specifies that new development will only be permitted where adequate, safe and functional provision is made for refuse collection. Details of the current guidelines are set out in the Waste and Recycling Storage Planning Guidance.

9.8.19 The submitted Transport Statement states that *“During discussion with the London Borough of Enfield Council about the provision of servicing from the site, the development proposes that servicing will take place from the existing carriageway rather than the previously proposed lay-by. A refuse collection point will be located adjacent to the Brettenham Road site entrance, with the refuse stores for both flats and the church located in the centre of the*

*development. Therefore, servicing and refuse collection will enable the preservation of the existing mature trees adjacent to the development and occur on-street.”* This approach is considered acceptable from a highway perspective and further details of refuse storage design and numbers could be secured by an appropriate condition.

#### Construction Management Plan

9.8.20 In order to ensure that construction traffic associated with the development can be accommodated without any adverse impacts on the surrounding local highway network; a ‘Construction Traffic Management Plan’ would be required, however details could be secured by an appropriate condition, having regard to policy DMD48 of the DMD.

9.8.21 The scale of the proposed development would require the provision of a temporary heavy duty crossover in order to facilitate construction plant (ready mix concrete lorries, flatbed delivery vehicles, grab lorries, skip lorries, etc.) accessing the site. A new (relocated) access is proposed off Brettenham Road as the footway at this point is not constructed to take any heavy vehicles. There is therefore a highway requirement to construct a heavy duty crossing that would support commercial vehicles over 3.5t. Under the existing footway construction, there may be utility apparatus laid at a shallow depth and this would need to be protected by the heavy duty crossing.

#### Sustainable Transport Contribution

9.8.22 Each new unit shall be entitled to a sustainable transport package which shall include car club membership for two years with £50 driving credit, an Oyster card per bedroom and two years of London Cycling Campaign Membership per bedroom. The applicant would be responsible for promoting the sustainable transport package and managing delivery. Confirmation would be required that the package has been offered to all first occupiers of residential units. This should be via an independent audit undertaken at the applicant’s cost. Where there is no evidence that the package has been offered to the first occupier of a residential unit, the applicant will be required to pay the relevant per unit contribution for all applicable units to the Council to support delivery of sustainable transport measures.

<b>Housing mix</b>	<b>Per unit contribution</b>
Studio / 1 bedroom	£306
2 bedroom	£474

#### Additional Highway Works

9.8.23 There would also be a requirement to undertake some S278 remedial work to the public highway as the old entrance off Brettenham Road needs to be reinstated as footway and a new access constructed. Road markings would also need to be refreshed and this is incorporated into the S106.

## 9.9 Trees, Landscaping & Biodiversity

- 9.9.1 A revised Arboricultural Impact Assessment (AIA) has been submitted in response to concerns initially expressed regarding the original proposals and the relationship to a number of street trees and their root system
- 9.9.2 A further update on tree matters will be provided for Members to consider in advance of the Planning Committee.
- 9.9.3 The application site is situated in a town centre environment and thus the site has little biodiversity or ecological value at present. This was supported by a Preliminary Ecological Appraisal submitted as part of the application. However, it is considered that the shunting of the building has provided some necessary open space to provide high quality soft landscaping to the frontage, including some tree planting and biodiversity enhancements. Further details of a landscaping scheme, including tree planting would be secured by an appropriate condition, having regard to policies DMD79, DMD80 and DMD81 of the DMD and CP36 of the Core Strategy. Additionally, a green roof could be integrated into the design to improve the appearance of the locality and biodiversity credentials within the urban setting, having regard to policy CP36 of the Core Strategy.
- 9.9.4 Developments resulting in the creation of 100m<sup>2</sup> of floorspace or one net dwelling or more should provide on-site ecological enhancements having regard to feasibility and viability.

## 9.10 Flooding and Drainage

- 9.10.1 Policy DMD59 states that new development must avoid and reduce the risk of flooding. Policy DMD61 of the Development Management Document states that a Drainage Strategy would be required for all new developments to demonstrate how proposed measures manage surface water as close to its source as possible and follow the drainage hierarchy in the London Plan. All development must maximise the use of, and where possible, retrofit Sustainable Drainage Systems (SuDS).
- 9.10.2 The Flood Risk Assessment was submitted as part of the supporting documentation as the site is located within a Flood Zone 2. The Council's SuDS officer provided comments relating to drainage during the course of the application to ensure compliance with the necessary Council requirements. However, it is recommended that appropriate conditions are attached to ensure that an appropriate SuDS strategy is secured and the approved drainage/SuDS details are fully implemented, having regard to policies CP28 of the Core Strategy, DMD59, DMD60 and DMD61 of the DMD and 5.12 & 5.13 of the London Plan as well as the advice contained within the NPPF

## 9.11 Sustainability

- 9.11.1 The revised National Planning Policy Framework (2019) places an increased emphasis on responding to climate change, having regard to long-term implications for flood risk, coastal change, water supply, biodiversity and landscape, and the risk of overheating from rising temperatures. New development can help to reduce greenhouse gas emissions, such as through its location, orientation and design. Any local requirements for the sustainability of buildings should reflect the Government's policy for national technical



standards and increase the use and supply of renewable and low carbon energy and heat. The NPPF states that even small-scale projects provide a valuable contribution to cutting greenhouse gas emissions.

- 9.11.2 Policy DMD49 states that all new development must achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. An energy statement in accordance with Policies DMD49 and 51 is required to demonstrate how the development has engaged with the energy hierarchy to maximise energy efficiency.
- 9.11.3 The London Plan adopts a presumption that all developments will meet carbon dioxide emission reductions that will improve upon 2010 Building Regulations, leading to zero carbon residential buildings from 2016. Policy 5.2 establishes a target for 2013 to be a 35% improvement over Part L of current Building Regulations
- 9.11.4 It was noted that an Energy and Sustainability Statement has been submitted, which details the following:
- High performance glazing is proposed throughout the development to reduce the effects of solar gain and to minimise overheating in summer and reduce heat loss during the winter;
  - gas-fired boilers will serve the church and individually for each apartment;
  - mechanical ventilation will be installed where appropriate; and
  - photovoltaic (PV) panels will be provided to the roofs of the building and assist in ensuring that a 35% reduction in carbon dioxide emissions is met in accordance with the London Plan. In addition, green roofs will be provided to other parts of the development.
- 9.11.5 Policy DMD55 requires all development to maximise the use of roof and vertical surfaces for Low and Zero Carbon Technology / Living Walls / Green Roofs. The submitted plans indicate that a green roof would be utilised as part of the design to the flat roof and this could be secured by an appropriate condition.
- 9.11.6 Policy DMD58 (Water Efficiency) expects new residential development, including new build and conversions, will be required to achieve as a minimum water use of under 105 litres per person per day. This could be secured by an appropriate condition.
- 9.11.7 Several conditions relating to climate change and sustainable design and construction have been suggested to address relevant policies within section 8 – Tackling Climate Change of the DMD.

## 9.12 Contamination

- 9.12.1 The revised NPPF refers to the need to enhance the natural and local environment by remediating and mitigating despoiled, degraded, derelict, contaminated and unstable land, where appropriate.
- 9.12.2 Having regard to past uses at the site, there is the potential for contamination at the site, which could pose a potential risk to human health. Environmental Health were consulted and have no objections to the proposed development subject to appropriate condition to deal with the potential for contamination at

the site as well as the control of dust and emissions, sound insulation, piled foundations, air quality, noise and a construction management plan, having regard to policies 5.3 and 7.14 of the London Plan, DMD64 of the DMD and CP32 of the Core Strategy.

### 9.13 CIL

9.13.1 This would be calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2012 and Enfield's adopted Community Infrastructure Levy Charging Schedule 2016. The payments would be chargeable on implementation of the residential development.

#### *Mayoral CIL*

9.13.2 The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought for the scheme is calculated on the net increase of gross internal floor area multiplied by the Outer London weight of £60 together with a monthly indexation figure. It is noted as of the 1<sup>st</sup> of April 2019 Mayoral CIL has increased to £60/m<sup>2</sup>

#### *Enfield CIL*

9.13.3 On April 2016, the Council introduced its own CIL and this scheme would be subject to an Enfield CIL rate of £40 per square metre.

9.13.4 The proposals would be liable for the Mayors and Enfield CIL contributions. Members will be updated on the CIL figures in advance of the planning committee.

## **10. Conclusion**

10.1 It is considered that the proposed development would on balance, be acceptable

10.2 Although it is recognised the proposals will incur the loss of the rear hall building which makes a positive contribution to the character of the Conservation Area, its replacement with a building of an acceptable design that preserves the landmark frontage building and the character of the Conservation Area, means that no harm would arise to the designated heritage asset as a result of the proposals in line with paras 193 and 197 of the NPPF.

10.3 Having regard to the above, the partial demolition and introduction of a replacement building that preserves the heritage asset and Fore Street Conservation Area as a whole is therefore considered acceptable, having regard to the advice contained within the National Planning Policy Framework, Planning (Listed Buildings and Conservation Areas) Act, 1990, policy CP31 of the Core Strategy, policy 7.8 of the London Plan and policy DMD44 of the DMD and the aims and objectives contained within the Fore Street Conservation Area Character Appraisal.

10.4 The heritage requirements have influenced the form and viability of the current scheme but it is noted that although there is no affordable housing, the development does provide an improved community facility as

well as delivering good quality housing stock towards the Boroughs housing targets within a town centre location.

- 10.5 The development would improve the local environment by delivering a greener frontage with landscaping and a green roof to enhance biodiversity and landscaping within a town centre location. Furthermore, the sustainability credentials for the building and site would be improved by the use of water efficiency techniques, measures to meet CO2 emissions and a sustainable drainage strategy;
- 10.6 The proposed development would not be detrimental to neighbouring amenity or have an unacceptable impact on highway function and safety.
- 10.7 Taking the above factors into consideration and subject to appropriate conditions and an S106 to secure appropriate highway contributions and matters pertaining to exclusion of residents obtaining parking permits, on balance, the scheme is considered acceptable. It is therefore recommended that planning permission is granted.

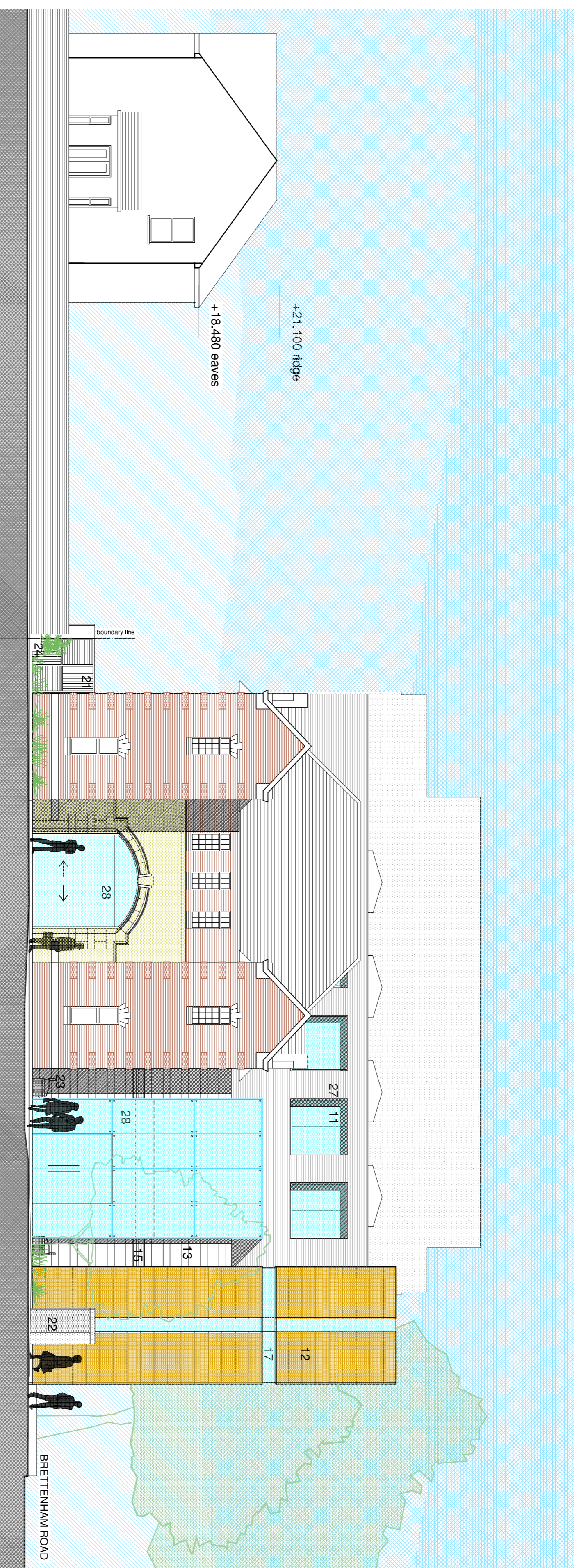


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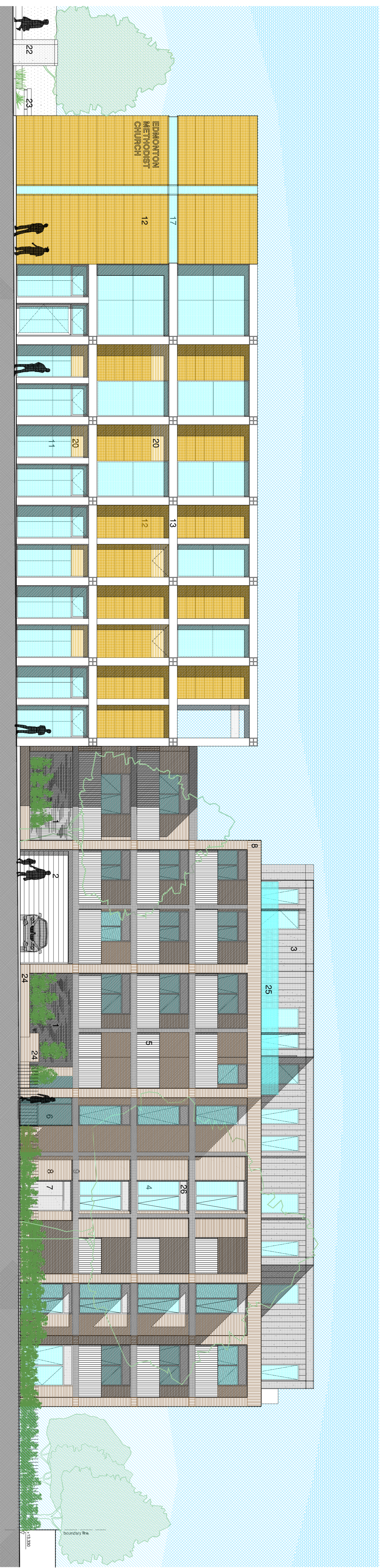
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A	21.10.19	West Elevation: entrance and glazed wall altered
B	20.12.19	Top floor altered: glazing added and materials altered to residential block. Windows altered. Description 27 added. Planning issue
C	28.01.20	Description 28 added
D	18.03.20	South Elevation: windows of residential building altered

Key to materials

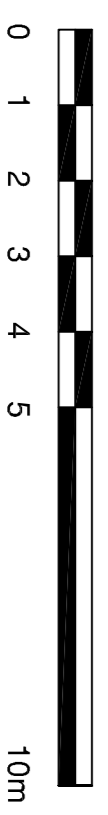
1. Decorative Galvanized Steel Screen
2. Power coated rolling grille
3. Pre-patinated zinc cladding panels - COLOUR T9C
4. Powder coated aluminium windows and doors
5. Steel balustrade
6. Powder coated aluminium entrance door
7. Powder coated steel door
8. Grey / brown brickwork - TBC
9. Pre-cast concrete panel
10. Cycle Store Glass Cover
11. Powder coated aluminium glazing system
12. Pre-patinated zinc cladding panels - COLOUR T9C
13. Reconstituted Stone
14. Powder coated louvred doors
15. Powder coated aluminium louvers
16. Powder coated aluminium window with opaque glass
17. Illuminated glass cross
18. New brickwork to match existing
19. Operable rooflights and PV panels
20. Pre-patinated zinc louvers
21. New galvanized steel gate
22. Existing telephone box
23. Concrete bench with timber top
24. Brick planters
25. Opaque glass balustrade
26. Powder coated aluminium panel
27. Light grey brickwork - TBC
28. Double glazed facade, part of structural glass system, Pilkington Planar, or similar approved. Refer to drawing SK122 for details.



West Elevations - Scale 1:100



South Elevations - Scale 1:100



PLANNING

Edmonton Methodist Church | Option 6 - Elevations - Sheet 1 of 2

● saville jones | consultants

DATE: AUG 2019 | 1414 SK 116D

SCALE: 1:100@A1



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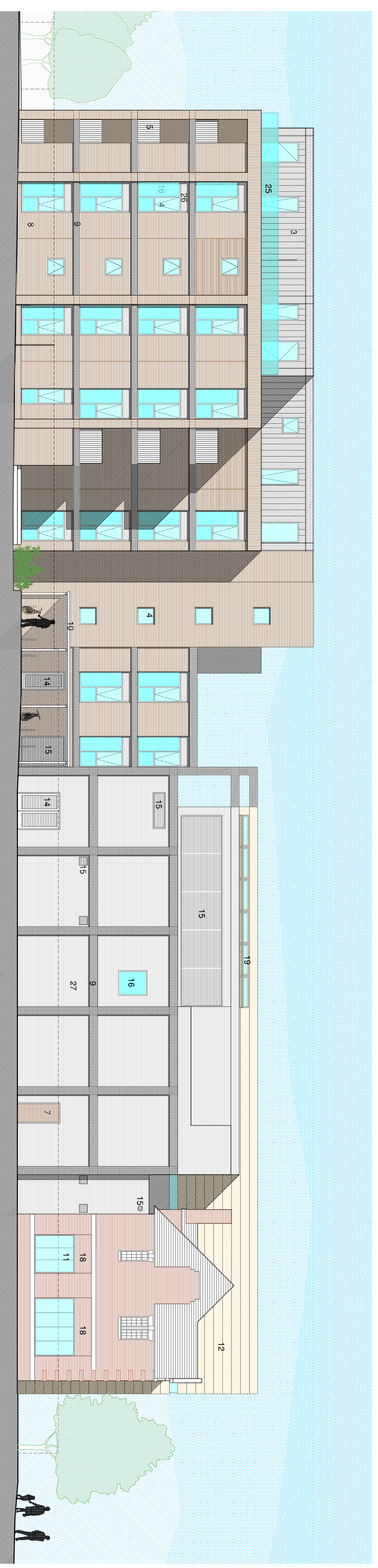
REV	DATE	DESCRIPTION
A	20.12.19	Top floor altered dimensions added and materials altered to residential block. Windows altered. Description 27 added. Planning Issue
B	28.01.20	East Elevation: clear glass windows changed to opaque glass windows and on window added to 4th floor. North Elevation: one window strip reduced to ground floor.
C	16.03.20	East Elevation: two windows added

**Key to materials**

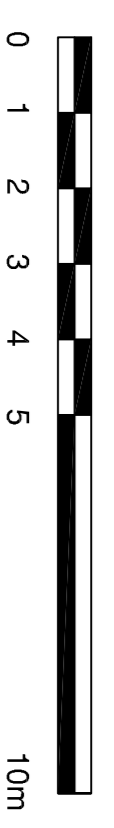
1. Decorative Galvanized Steel Screen
2. Power coated rolling grille
3. Pre-patinated zinc cladding panels - COLOUR TBC
4. Powder coated aluminium windows and doors
5. Steel balustrade
6. Powder coated aluminium entrance door
7. Powder coated steel door
8. Grey / brown brickwork - TBC
9. Pre-cast concrete panel
10. Cycle Store Glass Cover
11. Powder coated aluminium glazing system
12. Pre-patinated zinc cladding panels - COLOUR TBC
13. Reconstituted stone
14. Powder coated louvred doors
15. Powder coated aluminium louvres
16. Powder coated aluminium window with opaque glass
17. Illuminated glass cross
18. New brickwork to match existing
19. Operable rooflights and PV panels
20. Pre-patinated zinc louvres
21. New galvanized steel gate
22. Existing telephone box
23. Concrete bench with timber top
24. Brick planters
25. Opaque glass balustrade
26. Powder coated aluminium panel
27. Light grey brickwork - TBC



East Elevations - Scale 1:100



North Elevations - Scale 1:100



**PLANNING**

Edmonton Methodist Church Option 6 - Elevations - Sheet 2 of 2

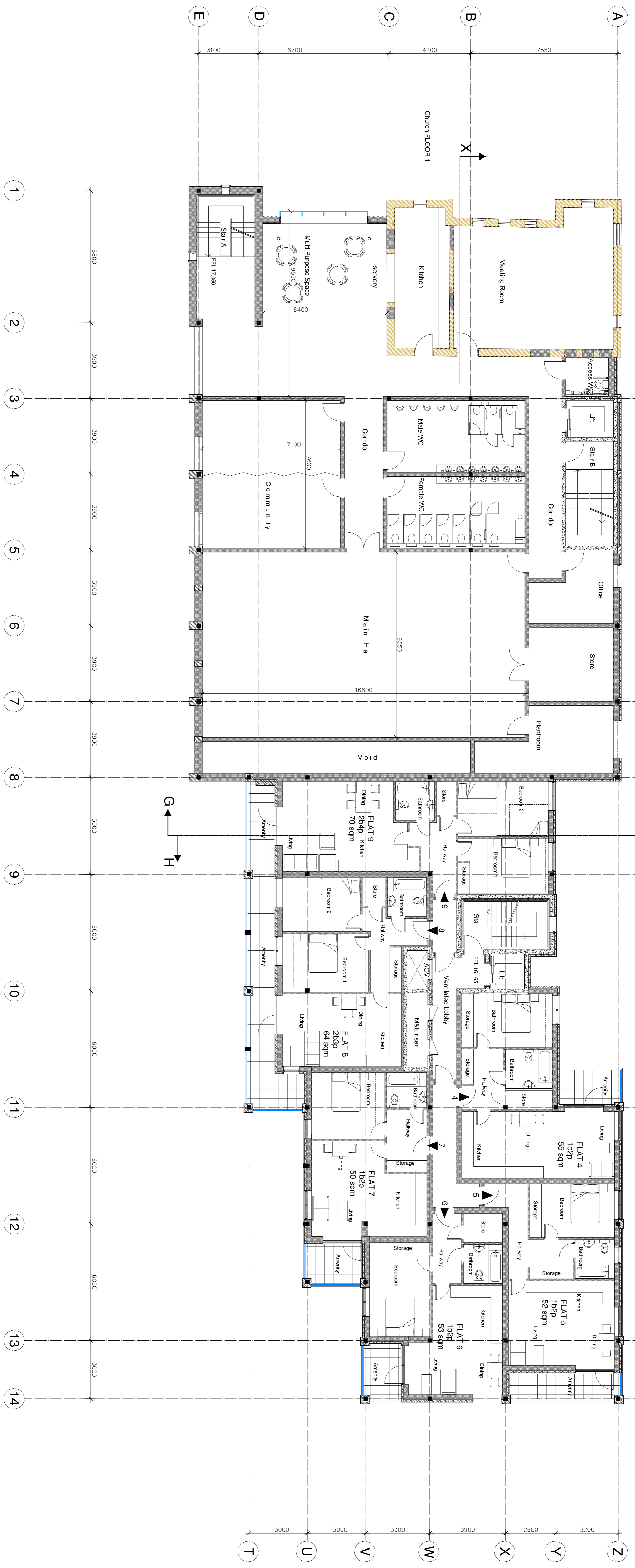
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DATE: Aug 2019  
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DRAWN: 1414 SK 117C

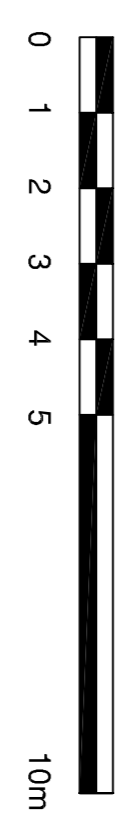
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 REV DATE DESCRIPTION  
 A 20.12.19 Stair and lift shaft altered Planning Issue  
 B 18.03.20 Flats 7 and 8 altered



First Floor Plan - Scale 1:100



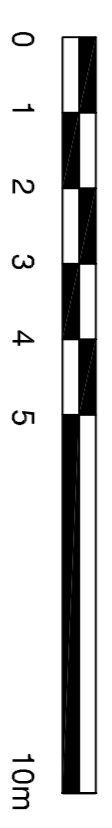
Schedule of Accommodation			
Flat number	Floor	Number of bedroom and lead spaces	Gross Internal Area
1	Ground	1b 2p	61 sqm
2	Ground	1b 2p	51 sqm
3	Ground	1b 2p	59 sqm
4	First	1b 2p	55 sqm
5	First	1b 2p	51 sqm
6	First	1b 2p	53 sqm
7	First	1b 2p	52 sqm
8	First	2b 3p	64 sqm
9	First	2b 4p	70 sqm
10	Second	1b 2p	55 sqm
11	Second	1b 2p	51 sqm
12	Second	1b 2p	53 sqm
13	Second	1b 2p	50 sqm
14	Second	2b 3p	54 sqm
15	Second	2b 4p	70 sqm
16	Third	1b 2p	55 sqm
17	Third	1b 2p	51 sqm
18	Third	1b 2p	53 sqm
19	Third	1b 2p	50 sqm
20	Third	2b 3p	67 sqm
21	Fourth	1b 2p	52 sqm
22	Fourth	1b 2p	50 sqm
23	Fourth	1b 2p	52 sqm
24	Fourth	1b 2p	53 sqm



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 REV DATE DESCRIPTION  
 A 20.12.19 Stair, lift shaft and flats altered. Planning issue  
 B 20.01.20 Windows altered to west side of Flat 24. One window added to west side of Flat 21. Bathroom window added to Flat 22. One window added to east side of Flat 23



Fourth Floor Plan - Scale 1:100



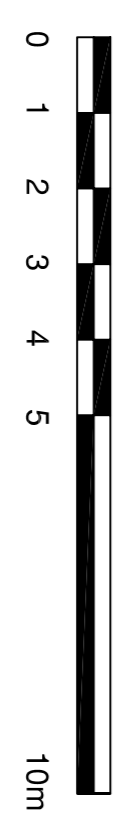
Flat number	Floor	Number of bedroom and bed spaces	Gross Internal Area	Other
1	Ground	1b 2b	61 sqm	Wheelchair User (M43) lit
2	Ground	1b 2b	51 sqm	Wheelchair User (M43) lit
3	Ground	1b 2b	59 sqm	Wheelchair User (M43) lit
4	First	1b 2b	55 sqm	-
5	First	1b 2b	51 sqm	-
6	First	1b 2b	53 sqm	-
7	First	1b 2b	50 sqm	-
8	First	2b 4b	51 sqm	-
9	First	2b 4b	70 sqm	-
10	Second	1b 2b	55 sqm	-
11	Second	1b 2b	51 sqm	-
12	Second	1b 2b	53 sqm	-
13	Second	1b 2b	50 sqm	-
14	Second	2b 3b	64 sqm	-
15	Second	2b 3b	70 sqm	-
16	Third	1b 2b	55 sqm	-
17	Third	1b 2b	51 sqm	-
18	Third	1b 2b	53 sqm	-
19	Third	1b 2b	50 sqm	-
20	Third	2b 3b	67 sqm	-
21	Fourth	1b 2b	52 sqm	-
22	Fourth	1b 2b	50 sqm	-
23	Fourth	1b 2b	50 sqm	-
24	Fourth	1b 2b	53 sqm	-



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 REV DATE DESCRIPTION  
 A 08.11.19 Planes, cycle spaces and paving altered  
 B 20.12.19 Stair and flat roof altered, paving issue  
 C 25.05.20 Electric Car Spaces added  
 D



Ground Floor Plan - Scale 1:100



**KEY**

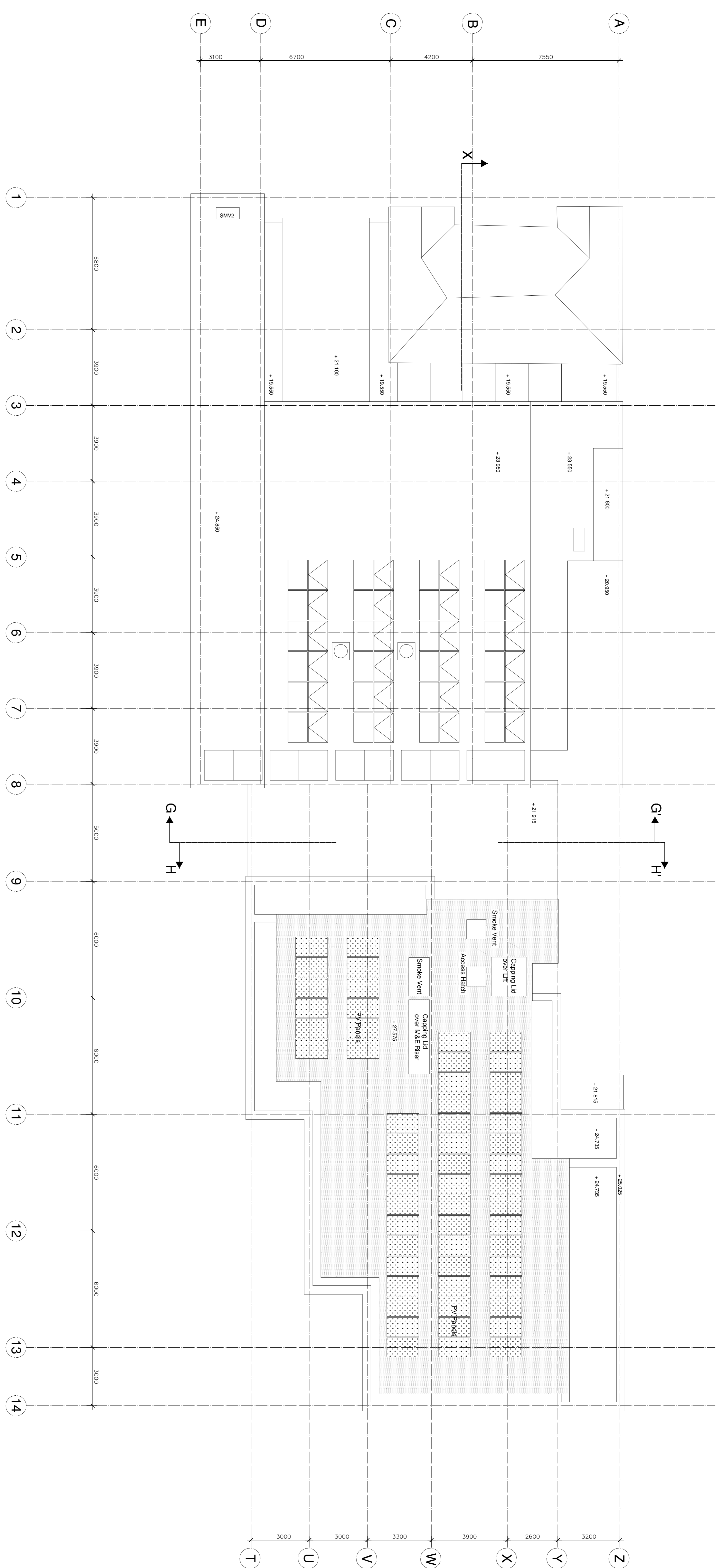
	Existing Building
	Grass / Landscaped area
	Approved Document M4(3): Category 3 - Wheelchair user flat
	Boundary Line
	Recyclable Waste - 660 litres bin
	Non-Recyclable Waste - 660 litres bin
	Food and Garden Waste - 660 litres bin
	Recyclable Waste - 240 litres bin
	Non-Recyclable Waste - 240 litres bin
	Food and Garden Waste - 240 litres bin
	Passive Electric car parking Space
	Active Electric car parking Space

**Schedule of Accommodation**

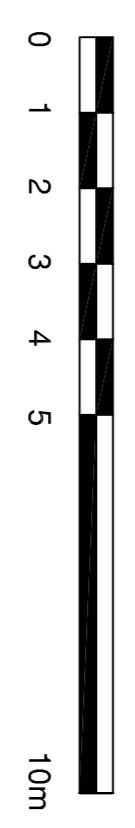
Flat number	Floor	Number of bedrooms and bed spaces	Gross Internal Area	Other
1	Ground	1b 2p	61 sqm	Wheelchair User M4(3) flat
2	Ground	1b 2p	51 sqm	Wheelchair User M4(3) flat
3	Ground	1b 2p	59 sqm	
4	First	1b 2p	55 sqm	
5	First	1b 2p	51 sqm	
6	First	1b 2p	53 sqm	
7	First	1b 2p	54 sqm	
8	First	2b 3p	64 sqm	
9	First	2b 3p	70 sqm	
10	Second	1b 2p	55 sqm	
11	Second	1b 2p	51 sqm	
12	Second	1b 2p	53 sqm	
13	Second	1b 2p	59 sqm	
14	Second	2b 3p	64 sqm	
15	Second	2b 4p	70 sqm	
16	Third	1b 2p	55 sqm	
17	Third	1b 2p	51 sqm	
18	Third	1b 2p	53 sqm	
19	Third	1b 2p	59 sqm	
20	Third	2b 3p	67 sqm	
21	Fourth	1b 2p	52 sqm	
22	Fourth	1b 2p	50 sqm	
23	Fourth	1b 2p	50 sqm	
24	Fourth	1b 2p	53 sqm	



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 REV DATE DESCRIPTION  
 A 20.12.19 Stair, lift shaft altered Planning Issue



Roof Plan - Scale 1:100



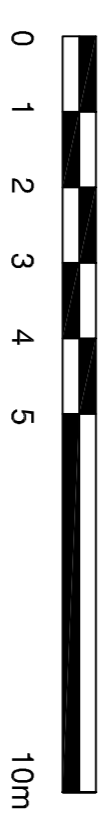
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DATE: Aug 2019  
 SCALE: 1:100 @A1  
 1414 SK 115A



Second Floor Plan - Scale 1:100



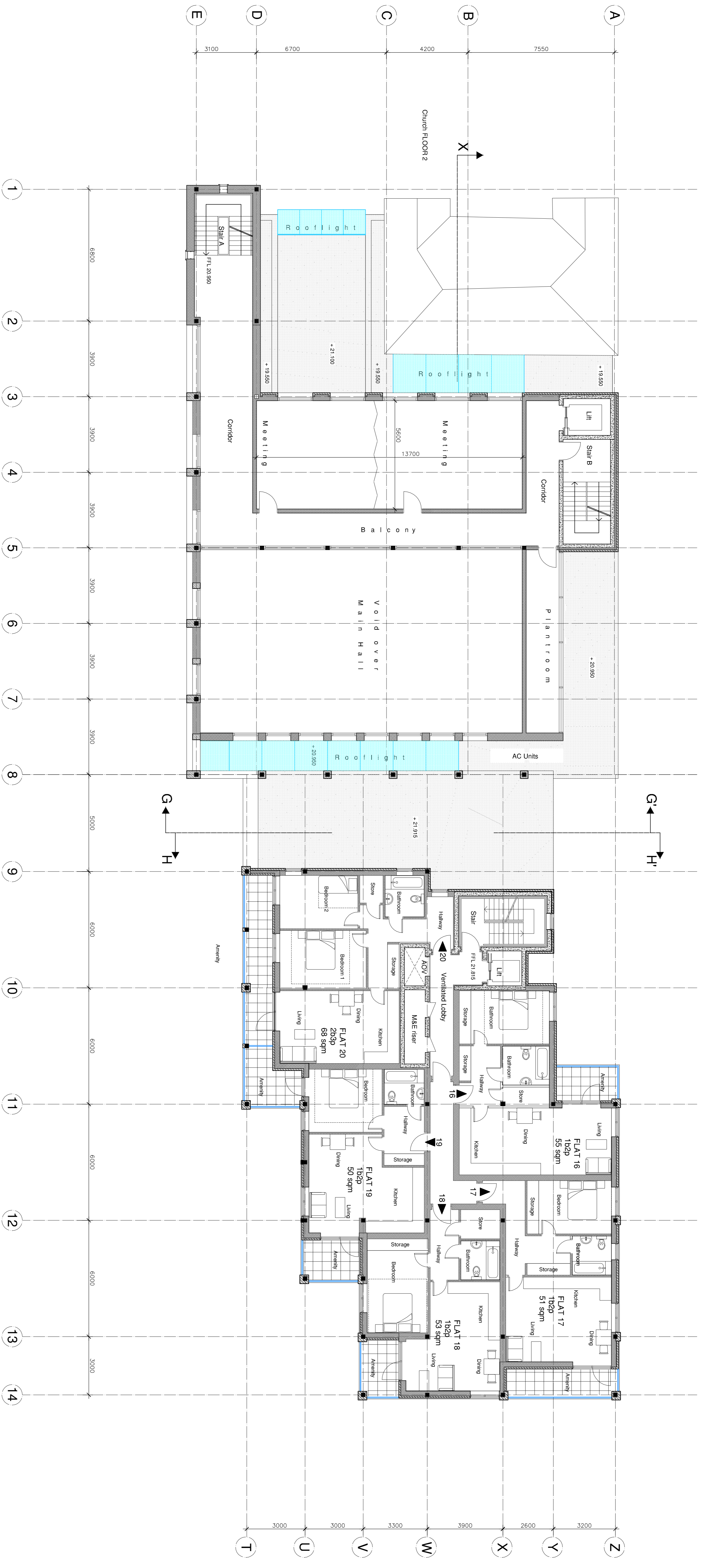
Flat number	Floor	Number of bedroom and sleep spaces	Gross Internal Area	Other
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2	Ground	1b 2p	51 sqm	Wheelchair User M4(3) list
3	Ground	1b 2p	59 sqm	Wheelchair User M4(3) list
4	First	1b 2p	55 sqm	.
5	First	1b 2p	55 sqm	.
6	First	1b 2p	53 sqm	.
7	First	1b 2p	50 sqm	.
8	First	2b 3p	64 sqm	.
9	First	2b 4p	70 sqm	.
10	Second	1b 2p	55 sqm	.
11	Second	1b 2p	51 sqm	.
12	Second	1b 2p	53 sqm	.
13	Second	1b 2p	50 sqm	.
14	Second	2b 3p	64 sqm	.
15	Second	2b 4p	70 sqm	.
16	Third	1b 2p	55 sqm	.
17	Third	1b 2p	51 sqm	.
18	Third	1b 2p	53 sqm	.
19	Third	1b 2p	50 sqm	.
20	Third	2b 3p	64 sqm	.
21	Fourth	1b 2p	52 sqm	.
22	Fourth	1b 2p	52 sqm	.
23	Fourth	1b 2p	50 sqm	.
24	Fourth	1b 2p	53 sqm	.

PLANNING

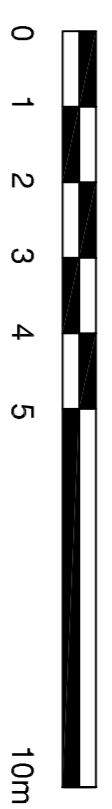


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REV DATE DESCRIPTION  
 A 20.12.19 Stair and lift shaft altered Planning Issue  
 B 28.01.20 Two windows added to west side of Flat 20  
 C 18.03.20 Flat 19 altered



Third Floor Plan - Scale 1:100



Flat number	Floor	Number of bedrooms and sq. metres	Gross Internal Area	Other
1	Ground	18.2p	61 sqm	Wheelchair User M4(3) lift
2	Ground	18.2p	51 sqm	
3	Ground	18.2p	59 sqm	Wheelchair User M4(3) lift
4	First	18.2p	55 sqm	
5	First	18.2p	51 sqm	
6	First	18.2p	53 sqm	
7	First	18.2p	50 sqm	
8	First	28.4p	64 sqm	
9	First	28.4p	70 sqm	
10	Second	18.2p	55 sqm	
11	Second	18.2p	51 sqm	
12	Second	18.2p	50 sqm	
14	Second	28.3p	64 sqm	
15	Second	28.4p	70 sqm	
16	Third	18.2p	55 sqm	
17	Third	18.2p	51 sqm	
18	Third	18.2p	53 sqm	
19	Third	18.2p	52 sqm	
20	Third	28.3p	67 sqm	
21	Fourth	18.2p	52 sqm	
22	Fourth	18.2p	50 sqm	
23	Fourth	18.2p	50 sqm	
24	Fourth	18.2p	53 sqm	

PLANNING

Edmonton Methodist Church

Option 6 - Third Floor Plan

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DATE: Aug 2019  
 SCALE: 1:100 @A1

1414 SK 1130

Doc: 04666201/0001/001/001/001



Rev.	Date	Description
A	25.01.18	Cross design altered
B	05.02.20	General revision



**PLANNING**

1414 SK53B

Edmonton Methodist Church

Proposed Visual 1 - West Elevation

Sep 2016

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Rev.	Date	Description
A	25.01.18	General revision
B	05.02.20	General revision



**PLANNING**

1414 SK54B

Edmonton Methodist Church

Proposed Visual 2 - South Elevation

Sep 2016

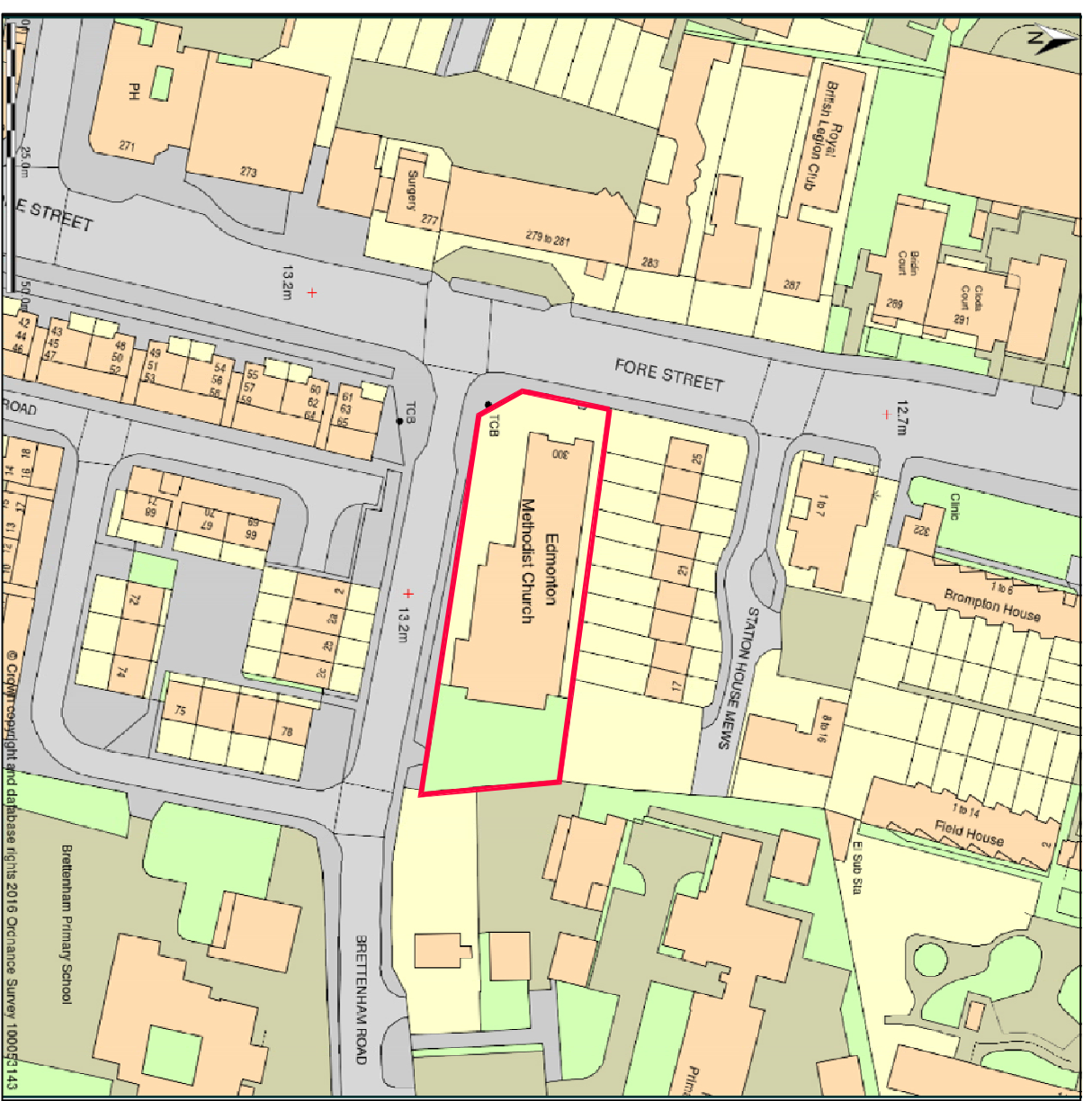
**saville jones** consultants





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REV DATE DESCRIPTION



Location Plan - Scale 1:1250

**PRELIMINARY**

Edmonton Methodist Church

Location Plan

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date: May 2016

scale: 1 :1250 @A3

1414 SK 16